

## Prospect Street Corridor Special Design District

### 1. Purpose

The purpose of the Prospect Street Corridor Special Design District (PSDD) is to encourage new development and redevelopment that is supportive of and promotes the formation of a mixed-use commercial and residential environment along Prospect Street as reflected in the 2013 Plan of Conservation and Development Future Land Use Plan. Additionally, the purpose is to encourage an economically vibrant corridor along Prospect Street with a dynamic mix of complementary uses that is also compatible with abutting and adjacent land uses.

### 2. Applicability

The PSDD is applicable to the area along Prospect Street as shown on the official zoning map. It includes all parcels with frontage on Prospect Street. The PSDD permits flexibility in the uses and standards within the zone and addresses special siting, height and bulk, use, design, and compatibility elements of development to meet the purposes of the zone.

The following applications within the PSDD are exempted from a Special Permit and may be considered for approval with a Site Plan application [\(see Section 32\)](#):

- Expansion of an existing building by less than 25 percent of the floor area
- Modification to an existing site plan which alters site design but does not:
  - Alter access locations or increase the number of driveways
  - Expand parking by more than 10%
  - Reduce total area of landscaping by more than 10%
- Single family residential uses
- Multi-family residential uses of ~~20-10~~ units or less
- Uses or mix of uses which require less than 10 parking spaces in total and do not include a drive-thru

All other allowable uses require approval with a Special Permit [and are subject to the required design standards and guidelines in Sections 4 through 11 below.](#)

### 3. Permitted Uses

Any use permitted in the following zones shall be permitted within the PSDD [\(see Schedule A\)](#):

- R-8 Residence
- RA-2 Residence
- RO-1 Residence Office
- Business District #1 Higher intensity/density commercial and office
- [Business District #2](#) [General commercial and office](#)
- Business District #3 Neighborhood oriented commercial
- Business District #4 Low-intensity commercial
- [Industrial #2](#) Light industrial/Research & Development facilities
- [RSC](#) [Regional Shopping Center](#)
- A mix of the above uses; [except that industrial and residential uses may not be mixed within the same development.](#)

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Except that the following uses are prohibited:

- Adult entertainment
- Earth excavation operations/ sand and gravel operations
- Public utility company electric power plants and generating facilities and gas storage
- Freight and materials, trucking businesses and terminals, bus maintenance terminals
- Bulk storage of cement and petroleum products, concrete mixing plants
- Junk yards and accessory junk yard uses
- Outdoor storage centers

#### 4. Design Review

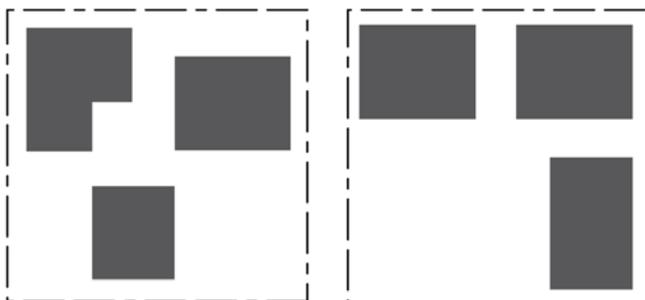
All applications for a Special Permit under the PSDD Zone shall be subject to review by the Advisory Architectural and Landscaping Review Board. Additionally, the proposed site plan which accompanies the application shall be subject to review by the following:

- Land use Office
- Borough Engineer
- Inland Wetlands Commission as applicable
- Planning Commission
- Naugatuck Valley Health District or Water Pollution Control Board as applicable
- Other technical professionals as the Commission may deem warranted and as specified in Section 59.17 of these regulations

#### 5. Site Layout

The organization of structures and landscape elements for any new development or any redevelopment should reinforce and contribute to an overall cohesive, human scale, and walkable environment in the PSDD. All efforts should be made to strengthen connections among parcels, create harmony of design, and preserve any unique landforms, notable built features such as stone walls, historic structures, scenic views, etc. and incorporate them into site design.

- For sites with multiple buildings proposed, efforts should be made to vary the footprint and alignment of buildings so as to provide visual interest (see example in Figure 1).
- Prominent site features (e.g. topography, rock, mature vegetation, water, etc.) should be preserved as feasible.
- Primary buildings shall be setback [from the Prospect Street property line](#) no less than 10 feet [or the height of the primary structure, as measured from ground level to cornice line on the center of the building face that abuts Prospect Street, whichever is greater.](#)
- ~~and no more than 60 feet from the Prospect Street property line.~~
- Side yard setbacks shall be a minimum of:
  - 10 feet from rear property line
  - 10 feet from side or other property lines
  - 20 feet from [a residential use or](#) residence district boundary line



Clustered buildings with varying footprints

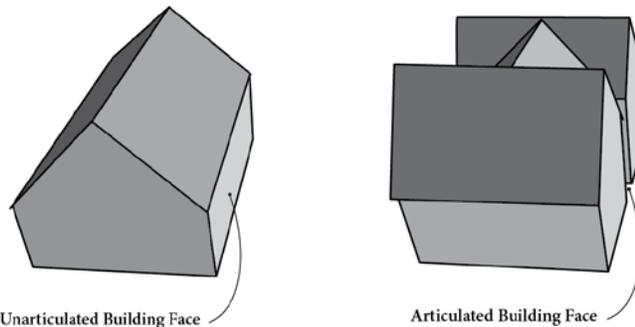
Linear arrangement of the same building footprint

**Figure 1. Building Footprint Examples**

## 6. Bulk and Height Requirements

Development in the PSDD is intended to be at a neighborhood scale. The height, footprint, overall dimensions, layout, and character of such structures should avoid a single big-box form with long generic undifferentiated walls along the street frontage.

- Buildings shall have a maximum of 4 stories and shall be a maximum of 50 feet in height as measured from ground level to cornice line on the center of the building face that abuts Prospect Street.
- Buildings with a footprint of greater than 20,000 sf are discouraged
- One story building structures are discouraged
- Each building fronting on Prospect Street should not exceed 200 feet in length along the street.
- Building structures with a footprint of 10,000 square feet or more should be divided into smaller sections and structures. This may be accomplished via the use of horizontal offsets, bumpouts, cross-gable features, and other architectural features and elements (see figure 2).
- In addition, blank wall surfaces (lacking doors or windows) greater than 50 feet in length should not be visible from Prospect Street or adjacent public streets.



**Figure 2. Building Composition**

## 7. Signage

Shall conform to the requirement of Section 27 of these regulations.

## 8. Parking

Off-street parking shall conform to the requirements of Section 26 of these regulations. Within the PSDD, additional requirements and flexible standards apply as follows:

- The number of parking spaces required for mixed-use developments shall be determined by the Borough Zoning Commission during the site plan review process. The requirement will be based upon the parking demand analysis submitted by the applicant.
- ~~Single family residential uses are exempted from these requirements~~
- Only retail and restaurant uses are permitted to have parking in front of the building. Parking lots in front of buildings shall not exceed a single row of parking. Excepting gas stations, ~~44~~ maximum pavement width (measured perpendicular to the front of the building) of parking areas in front of buildings shall be limited to ~~40-44~~ feet.

- Parking, except as noted for retail and restaurant uses, shall be located behind or on the side of buildings that front Prospect Street. Side parking lots shall not extend beyond the building line as established by the Prospect Street face of the primary structure.
- ~~• Where two or more different uses occur on a single lot, the Commission may approve the joint use of parking space by two or more establishments on the same or on contiguous lots, the total capacity of which space may be less than the sum of the spaces required for each, provided:
 
  - a. The Commission finds that the proposed capacity shall meet the parking demand as demonstrated by a parking analysis provided by the applicant and which meets the intents of these regulations;
  - b. Legal documentation shall be furnished establishing such rights for the duration of such joint use.~~
- There shall be one (1) secured bicycle parking space per multi-family dwelling unit. There shall be one (1) secured bicycle parking space per 3,000 sq. ft. of non-residential space or 10% of the number of automobile spaces (whichever is greater) .
- Bicycle racks should be located along a major building approach line and clearly visible from the approach.

## 9. Site Circulation

Driveways shall be located and designed to minimize vehicle, pedestrian, and bicyclist conflicts. Within the PSDD, added requirements apply as follows:

- Sidewalks or pathways shall be provided along the Prospect Street frontage. Where physical site conditions make a sidewalk along the Prospect Street frontage infeasible, sidewalk connections within the parcel to adjacent parcels may be considered by the Zoning Commission as an acceptable alternative.
- ~~•~~
- Sidewalks should be provided to connect all parking areas and building structures; sites should be laid out to maximize pedestrian connectivity between uses and sites.
- Potential conflict points between pedestrians and motor vehicles should be minimized.
- ~~• Properties with less than 200 feet of frontage on Prospect Street shall have a maximum of one driveway.~~
- ~~• Properties with 200 feet of frontage or more on Prospect Street may have a maximum of two driveways providing the driveways are separated by at least 100 linear feet at their intersection with Prospect Street.~~
- Emergency access may be provided by driveway connections through adjacent properties.
- Drive-thru circulation should be entirely within a property and should avoid separate access points from Prospect Street or other public streets.
- Cross access among and connecting adjacent parcels (for both vehicles and pedestrians) and shared driveways are encouraged.
- ~~• Single family residential driveways are limited to 20' in width at intersection with Prospect Street or adjoining public streets.~~
- Driveways shall be no more than 24' wide, excluding areas required for turning. Turning requirements for design vehicle shall be demonstrated by applicant.
- ~~• Maximum driveway width at the intersection with Prospect Street, including pavement area required by turning radius, shall be 40 feet.~~

- Driveways ~~, excluding single family residential uses,~~ shall be bordered by curbing unless drainage requirements require the absence of a curb.
- ~~Visibility at the intersection of a driveway with~~ Driveway location and design on Prospect Street or an adjoining public street shall conform with ~~sight-line~~ requirements as established by the Connecticut Department of Transportation's Highway Design Manual.
- The intersection of private driveways shall be sufficient to allow a stopped vehicle to see and be seen from a vehicle approaching from either direction.

## 10. Landscaping and Open Space

Any portion of a developed lot that is not used for the location of buildings, structures, accessory uses, outside storage areas, off-street parking and loading areas, sidewalks or other paved areas, shall be landscaped. Landscaping should include a variety of plants including shrubs, trees, and groundcovers.

Landscaping within the PSDD shall conform with the General Landscaping Requirements established in Section 33.2.4 of these regulations. Additionally, the following standards apply within the PSDD:

- A minimum of 10%, ~~exclusive of undisturbed naturalized areas,~~ of the lot developed area shall be landscaped.
- ~~The use of invasive species (as identified by CT DEEP) is prohibited. The use of native plant species is encouraged.~~
- Vegetated areas should be designed to integrate Low Impact Development stormwater techniques as feasible.
- No plant should be located to create – nor grown to create – a visual hazard for vehicular or pedestrian traffic either within or at the intersection of the site's access with a street.
- Fences should be designed to avoid barriers to pedestrian connections.
- ~~Parking areas containing 20 or more spaces shall have landscape coverage of at least 30% of the gross parking lot area. Required landscaping shall be placed within 20 feet of the parking area perimeter and/or within the parking area. The gross parking lot area shall be established by a calculation of the area within the outside perimeter of all parking spaces, excluding connecting driveways and landscaped islands. (See figure 3)~~
- ~~Parking areas with 40 or more parking spaces shall have at least 10% of the required landscaping within the paved portion of the lot.~~
- ~~Parking areas shall be bordered by evergreen landscaping so as to buffer the view from Prospect Street, adjacent public streets, and/or adjacent properties.~~

## 11. Energy Efficient Design

- Green design features should be considered and may include "green roofs" on buildings (natural plantings), use of permeable paving materials, and alternative energy sources such as solar panels.
- Without adversely affecting the reasonable solar gain of any neighboring property, new plantings should concentrate evergreen trees and hedges on the north side of a structure to protect from wind without interfering with passive solar gain; and deciduous planting on the south side for seasonal sun control.
- Orient the building consistent with energy conservation principles as feasible.

- Wherever possible, new technologies that reduce energy usage are encouraged including solar light fixtures, etc.
- LEED standards (Leadership in Energy and Environmental Design) are encouraged in the construction of buildings

## **12. Site Plan Requirements**

In addition to the requirements set forth in Section 32 and 33 of these regulations, an application for a Special Permit within the PSDS on lots of 5 acres or more shall specifically include:

- a. A comprehensive traffic study. The following information should be included:
  - (1) Existing and projected background traffic counts on major streets located in and adjacent to the SDD
  - (2) Analysis of anticipated traffic to be generated by the land uses proposed for the SDD, including projected levels of service and queuing at key intersections
  - (3) Description of traffic improvements, including pedestrian and public transit improvements, to mitigate traffic impacts
  - (4) Anticipated phasing of traffic improvements
- b. A parking demand analysis
- c. A comprehensive stormwater drainage study. The following information should be included:
  - (1) Analysis of existing and proposed peak rates of stormwater discharge from the SDD
  - (2) Description of stormwater drainage improvements to be constructed, including phasing

### 13. Waivers

The Zoning Commission may, by a minimum fourth fifths vote of all members of the Commission waive the standards for site design as required above and/or the submission of all or part of the information required by any part of this section of the Zoning Regulations if it finds:

- That physical conditions unique to the site would make strict adherence to one or more of the design standards infeasible.
- That the design as proposed by the applicant can still meet or exceed the intents and purposes of the zone.
- That the proposed design would provide benefits to the Borough community greater than that obtainable by strict adherence to one or more of the design standards, OR
- That the information is not necessary in order to decide on the application for a Special Permit within the PSDD.

### 14. Application Procedures

Any application for development within the PSDD shall be submitted in accordance with Section 52 of these regulations. Any use requiring a Special Permit within the PSDD shall be made under the Special Permit procedures set forth in these regulations.

#### *Pre-Application Meeting*

Prior to submission of an application for development under the PSDD, the Applicant shall meet with Borough staff and the Commission to review the proposed site for the project and subsequent site plans for each submitted phase. The pre-application meeting shall be held no less than 60 days prior to submission of a full application. Following the pre-application review, the Commission may retain technical professionals in accordance with Section 59.17 of these regulations to provide a review of the proposed site plan and subsequent site plans and studies required as part of the PSDD application.

### 15. Modification To PSDD Boundary

Any proposed modification to the PSDD boundary shall be considered a zone change request and the requirements for a Zone Change Application shall apply.

In addition to the language proposed above – an additional adjustment to another section of the regulations should be made as follows:

[Section 33.2.4.](#)

[Landscaping, Screening and Buffer Areas In New Haven Road And Rubber Avenue Design Districts and Prospect Street Design District](#)