

Prospect Street Corridor Special Design District

1. Purpose

The purpose of the Prospect Street Corridor Special Design District (PSDD) is to encourage new development and redevelopment that is supportive of and promotes the formation of a mixed-use commercial and residential environment along Prospect Street as reflected in the 2013 Plan of Conservation and Development Future Land Use Plan. Additionally, the purpose is to encourage an economically vibrant corridor along Prospect Street with a dynamic mix of complementary uses that is also compatible with abutting and adjacent land uses.

2. Applicability

The PSDD is applicable to the area along Prospect Street as shown on the official zoning map. It includes all parcels with frontage on Prospect Street. The PSDD permits flexibility in the uses and standards within the zone and addresses special siting, height and bulk, use, design, and compatibility elements of development to meet the purposes of the zone.

Applications within the PSDD may be considered for approval with a Site Plan application for the following conditions:

- Expansion of an existing use or mix of uses of 25 percent or less
- Modification to an existing site plan which alters site design but does not alter access locations or increase number of driveways, reduce landscaping, expand parking by more than 10%, or modify existing structures
- Single family residential uses
- Uses or mix of uses which collectively require less than 20 parking spaces and do not include a drive-through
- Multi-family residential uses with 20 units or less

All other allowable uses require approval with a Special Permit

The PSDD is applicable to any lot within the overlay zone without regard to minimum lot size.

3. Permitted Uses

Any use permitted in the following zones shall be permitted within the PSDD:

- R-8 Residence
- RA-2 Residence
- RO-1 Residence Office
- Business District #1 Higher intensity/density commercial and office
- Business District #3 Neighborhood oriented commercial
- Business District #4 Low-intensity commercial
- Industrial #2 Light industrial/Research & Development facilities
- A mix of the above uses

Except that the following uses are prohibited:

- Gas stations, auto repair and auto-body shops, and automobile sales

- Adult entertainment
- Heavy manufacturing, warehousing
- Earth excavation operations/ Sand and Gravel operations

4. Design Review

All applications for a Special Permit under the PSDD Zone shall be subject to review by the Advisory Architectural and Landscaping Review Board. Additionally, the proposed site plan which accompanies the application shall be subject to review by the following:

- Land use Office
- Borough Engineer
- Inland Wetlands Commission as applicable
- Planning Commission
- Naugatuck Valley Health District or Water Pollution Control Board as applicable
- Other technical professionals as the Commission may deem warranted and as specified in Section 59.17 of these regulations

5. Site Organization

The organization of structures and landscape elements for any new development or any redevelopment should reinforce and contribute to an overall cohesive, human scale, and walkable environment in the PSDD. All efforts should be made to strengthen connections among parcels, create harmony of design, and preserve any unique landforms, notable built features such as stone walls, historic structures, scenic views, etc. and incorporate them into site design.

- Site variety should be created using building clustering, and open space breaks.
- For sites with multiple buildings proposed, efforts should be made to vary the footprint among the building forms to minimize the overall sense of mass of the collective buildings (see example in Figure 1).
- Prominent site features (e.g. topography, rock, mature vegetation, water, etc.) should be used to organize the architectural and site composition.
- Buildings along a public street should generally be built to meet the property line (see example in Figure 2).
- Buildings should be no more than 10' from the front property line unless public space is provided in order to add to the overall streetscape appearance.
- Side yard setbacks may be a minimum of:
 - Zero feet from front property line at the street
 - 10 feet from rear property line
 - 10 feet from side or other property lines
 - 15 feet from residence district boundary line
- Maximum building coverage on a lot may be up to 80%

Figure 1. Building Footprint Examples

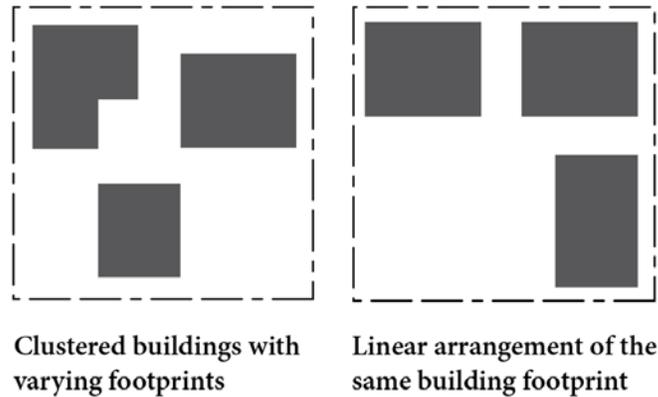


FIGURE 2 PENDING

6. Bulk and Height Requirements

Development in the PSDD is intended to be at a neighborhood scale. The height, footprint, overall dimensions, layout, and character of such structures should not suggest a single big-box with long generic undifferentiated walls along the street frontage.

- Buildings up to 4 stories are permitted
- Buildings with a footprint of 42,000 square feet or less are permitted; buildings with a footprint of 15,000 square feet or less are encouraged
- One story building structures are discouraged
- Each building fronting on Prospect Street should not exceed 200 feet in length along the street.
- Avoid long, large, unarticulated structures; as they do not add visual interest to the streetscape or surrounding landscapes.
- Larger structures should be broken into smaller sections and structures. This may be accomplished via the use of horizontal offsets, bumpouts, cross-gable features, and other architectural features and elements
- In addition, blank wall surfaces greater than 50 feet in length should not be visible from streets or other public areas.

7. Signage

Shall conform to the requirement of Section 27 of these regulations

8. Parking, Sidewalks, and Bicycle Access

Off-street parking shall conform to the requirements of Section 26 of these regulations. Within the PSDD, additional requirements and flexible standards apply as follows:

- Parking should be located behind buildings fronting on the street. Parking along the side of buildings may also be acceptable.
- Where there is a view of parking lots from a street and/or private accessway, the parking lot/spaces should be buffered with landscaping that is green year round.
- Sidewalks and pathways should be provided along the street frontage and within and among all parking areas; sites should be laid out to maximize pedestrian connectivity between uses and sites.
- Potential conflict points between pedestrians or bicyclists and motor vehicles should be minimized.
- There shall be one (1) secured bicycle parking space per dwelling unit. There shall be one (1) secured bicycle parking space per 3,000 sq. ft. of non-residential space or 10% of the number of automobile spaces (whichever is greater) .
- Bicycle racks should be provided and located along a major building approach line and clearly visible from the approach.
- All uses which are required to provide twenty (20) or more off-street parking spaces should have at least 40% of the gross parking lot area landscaped and with at least 10% of that landscaping in the lot interior, paved portion of the lot. (See Figure 3)
- Where two or more different uses occur on a single lot the Commission may approve the joint use of parking space by two or more establishments on the same or on contiguous lots, the total capacity of which space may be less than the sum of the spaces required for each, provided:
 - a. The Commission finds that the proposed capacity shall meet the parking demand as demonstrated by a parking analysis provided by the applicant and which meets the intents of these regulations;
 - b. Legal documentation shall be furnished establishing such rights for the duration of such joint use.

FIGURE 3 PENDING

9. Access and Driveways

Accessway and driveways shall be located and designed to minimize vehicle, pedestrian, and bicyclist conflicts. Within the PSDD, additional requirements apply as follows:

- Sites should be served by no more than one driveway
- Where a property has more than 300 feet of frontage on a public street – two driveways separated by at least 200 feet may be acceptable
- Emergency access may be provided by drives or accessway connections through adjacent properties.
- Drive-through circulation should be entirely within a property and should avoid separate access points onto a public street
- Cross access among and connecting adjacent parcels (for both vehicles and pedestrians) and shared driveways are encouraged
- Driveway or private accessway widths should be no more than 26 feet.

- Driveways should be clearly defined with curbing and the balance of property frontage on a street landscaped.
- Visibility at the intersection of any access drive with any intersecting street shall be sufficient to allow a stopped vehicle to see and be seen from a vehicle approaching from either direction.

10. Landscaping and Open Space

Any portion of a developed lot that is not used for the location of buildings, structures, accessory uses, outside storage areas, off-street parking and loading areas, sidewalks or other paved areas, should be landscaped. Landscaping should be sustainable and include a variety of plants including shrubs and trees to create interest, color, and texture. Landscaping should integrate the proposed development to the site, with consideration for natural topography and existing vegetation.

- A minimum of 10% of the lot shall be landscaped.
- Landscaping should be provided around buildings to establish continuity within the site, soften structure harshness, and introduce pedestrian scale at the sidewalk level.
- Use of native species for landscaping is encouraged
- Vegetated areas should be designed to integrate Low Impact Development stormwater techniques. Rain gardens can be used to collect run-off. Drainage areas and culverts can provide habitat areas when appropriately planted.
- No plant should be located to create – nor grown to create – a visual hazard for vehicular or pedestrian traffic either within or at the intersection of the site's access with a street.
- Fences should be designed to avoid barriers to pedestrian connections.
- Fences should be used to visually reinforce a space, add a decorative element, or provide a screen as required elsewhere in the zoning regulations and may be covered with vegetation.

11. Energy Efficient Design

- Green design features should be considered and may include green roofs on buildings (natural plantings), use of paving materials which permit natural absorption, and alternative energy sources.
- Without adversely affecting the reasonable solar gain of any neighboring property, new plantings should concentrate evergreen trees and hedges on the north side of a structure to protect from wind without interfering with passive solar gain; deciduous, on the south side for seasonal sun control.
- Orient the building consistent with energy conservation principles as feasible.
- Wherever possible, new technologies that reduce energy usage are encouraged including solar light fixtures, etc.
- LEED standards (Leadership in Energy and Environmental Design) are encouraged in the construction of buildings

12. Site Plan Requirements

In addition to the requirements set forth in Section 32 and 33 of these regulations, an application for a Special Permit within the PSDD on lots of 5 acres or more shall specifically include:

- a. A comprehensive traffic study. The following information should be included:
 - (1) Existing and projected background traffic counts on major streets located in and adjacent to the SDD
 - (2) Analysis of anticipated traffic to be generated by the land uses proposed for the SDD, including projected levels of service and queuing at key intersections
 - (3) Description of traffic improvements, including pedestrian and public transit improvements, to mitigate traffic impacts
 - (4) Anticipated phasing of traffic improvements
- b. A parking demand analysis
- c. A comprehensive stormwater drainage study. The following information should be included:
 - (1) Analysis of existing and proposed peak rates of stormwater discharge from the SDD
 - (2) Description of stormwater drainage improvements to be constructed, including phasing

13. Waivers

The Zoning Commission may, by a minimum fourth fifths vote of all members of the Commission waive the standards for site design as required above and/or the submission of all or part of the information required by any part of this section of the Zoning Regulations if it finds that the information is not necessary in order to decide on the application for a Special Permit within the PSDD.

14. Application Procedures

Any application for development within the PSDD shall be submitted in accordance with Section 52 of these regulations. Any use requiring a Special Permit within the PSDD shall be made under the Special Permit procedures set forth in these regulations.

PRE-APPLICATION MEETING

Prior to submission of an application for development under the PSDD, the Applicant shall meet with Borough staff and the Commission to review the proposed site for the project and subsequent site plans for each submitted phase. The pre-application meeting shall be held no less than 60 days prior to submission of a full application. Following the pre-application review, the Commission may retain technical professionals in accordance with Section 59.17 of these regulations to provide a review of the proposed site plan and subsequent site plans and studies required as part of the PSDD application.

15. Modification To PSDD Boundary

Any proposed modification to the PSDD boundary shall be considered a zone change request and the requirements for a Zone Change Application shall apply.